STONEGATE PLANNED AREA DEVELOPMENT AMENDMENT STANDARDS AND GUIDELINES REPORT

Stonegate - City of Maricopa, AZ

PAD24-05 Major Amendment (PAD23-05) Minor Amendment (PAD20-05) Major Amendment (PAD19-02) Major Amendment (PAD08-01) Original Approval



Submitted on Behalf of:

Dominium, Inc.
Development, Mountain West Region
9000 E. Pima Center Parkway, Suite 350
Scottsdale, Arizona 85258

Principals and Development Team

Property Owners:

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Applicant/Developer:



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Introduction

This standards and guidelines report has been prepared to support the Major PAD Amendment for approximately __ acres (Pinal County Assessor's Parcel Numbers 512417200, 512417210, 512417220, 512417230, 512417240, 512417250, 512417260, 512417270) located at the northwest corner of Maricopa-Casa Grande Highway and Stonegate Road in the City of Maricopa (the "Site"). The Site is bordered by Stonegate Road to the southeast, Maricopa-Casa Grande Highway to the southwest, undeveloped land to the west, Alan Stephens Parkway to the northeast, and the Willow single-family residential neighborhood to the north. The Site is an area designated for Mix-Use and development per City of Maricopa General Plan.



Aerial Map

Legal Description of Property and Parcels

Lots 1-8 of the Final Plat for Stonegate.

2024 Proposed PAD Amendment

The proposed Major PAD Amendment will allow for reduction of the "spine road" bisecting the Site. The "spine road" previously approved is no longer applicable or logical with the recent approval of the Home Depot/future retail pads along Maricopa-Casa Grande Highway frontage and Stonegate Road. Dominium is proposing build-to-rent housing onsite as envisioned in 2020, and an alternative to the "spine road" is being proposed as well as modified development standards to facilitate this compatible housing type. The BTR proposal will be owned and managed by Dominium - one of the nation's largest and most innovative development companies focused on workforce housing. It should be noted, reducing the "spine road" previously approved

creates less traffic congestion along Alan Stephens Parkway as well as potential cut through traffic.

Dominium's proven approach is to focus on meeting the needs of any given community by fulfilling a need for housing while also providing excellent living experience for its residents. Dominium takes pride in designing architecturally beautiful communities with high-quality finishes and amenities and providing excellent customer service that create long-term value for its residents, cities, and its financial stakeholders. Dominium's superior reputation is built on a 50-year track record in developing and managing residential rental communities. Dominium looks forward to being a City of Maricopa property owner and neighbor while providing desperately needed, unique, diverse, quality, and affordable rental communities.

The purpose of this Major PAD Amendment is to facilitate both Dominium's BTR development and the remaining development on the Stonegate PAD property. The Major PAD Amendment still emphasizes the "living experience" of the mixed-use development while still highlighting walkability and the pedestrian and cyclist experience, interconnectivity with uses onsite/offsite, quality/appropriate building scale, and a visually interesting internal streetscape/internal pedestrian trail connection. This Major PAD Amendment does not change the flexibility built into the 2020 previous approval of potential additional uses that may be built in the future, such as retail: multifamily (garden style) apartments, offices, restaurants, convenience store with gas, etc.



Planned Land Uses Map

Summary of Changes PAD24-05 Major Amendment

A. Spine Road

The proposed PAD Amendment will truncate the previously approved "spine road" that was planned to connect Maricopa-Casa Grande Highway and Altan Stephens Parkway. The former "spine road" is no longer applicable or logical with the recent approval of the Home Depot/future retail pads along Maricopa-Casa Grande Highway frontage and Stonegate Road. A design alternative to the "spine road" is being proposed that will facilitate Dominium's BTR development and the proposed future multifamily rental development site remaining. Where the truncated road ends, an internal multi-use trail connection will continue to connect to Alan Stephens Parkway and facilitate (i.e., where the previous "spine road" was located) a more pedestrian and bike friendly/walkable mix of uses within the Maricopa Stonegate PAD.

B. Development Standards

The purpose of this Major PAD Amendment is to facilitate both Dominium's BTR development and the remaining development on the Stonegate PAD property. The Major PAD Amendment will establish development standards specific to the BTR product type.

C. Scriveners' Errors

The PAD document has been revised to address minor scriveners' errors that have carried forward through prior iterations. The limited revisions will improve the readability of the overall document.



Maricopa Stonegate Mixed Use Development (PAD24-05)



Maricopa Stonegate PAD (as amended)

Alan Stephens Parkway & Stonegate Road

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Maricopa Stonegate PAD Amendment

Alan Stephens Parkway & Stonegate Road

Introduction:

Maricopa Stonegate will develop as a loosely integrated, suburban, mixed-use community that utilizes pedestrian systems, architecture, landscaping, and streetscape themes as common unifying elements. These elements will be combined and integrated in a way that will develop a unique sense of place within the development and set it apart from typical suburban development. The project will address current development trends towards more pedestrian-friendly development environments while respecting the suburban nature of the surrounding area. The development of Maricopa Stonegate will fulfill the General Plan requirements of a "village" which will service the surrounding master-planned communities. The 2040 General Plan gives great emphasis on the interconnectivity of new development with the surrounding community. The benefits:

- 1. Accessible mixed-use center with amenities useable by the community by pedestrian, bicycle, and auto.
- 2. Creating a "people place" where auto traffic is not the primary means of transportation
- 3. Creating a diverse sustainable development
- 4. Creating a village with a mixture of single family, multi-family, and commercial development easily accessible and walkable from the surrounding residential communities.
- 5. Diversity and changeability: As shown on page 9, Proposed Land Use Plan, the proposed development is diverse and will have the added ability for change to meet the constantly changing needs of the community. While all the uses shown are conceptual, the core idea is to create a village. The multi-family may change to professional offices, retail, or other compatible uses. The large box retail is flexible to a variety of larger scale uses, such as a theater complex, traditional retail, or office.

This zoning request will change the zoning to a planned area development district (PAD). The zoning will allow for a mixed- use neighborhood "village" development. This development will have a single family and a multifamily component. Other uses may be entertainment, retail, offices, restaurants, hotel, and convenience store with gas. These uses may be wrapped into one zoning classification (PAD) allowing flexibility and creating a "village" type development within existing master-planned communities. Previously, the property was zoned for Multi-family Residential. The land was subsequently rezoned to C1-1 for Light Industry and Warehouse Zone.

The following overriding design objectives will guide development on the site:

- Site design will thoughtfully integrate each independent land use into a continuous flow of activity while respecting the site context and suburban residential nature of the surrounding area.
- Horizontal integration of the land uses will be achieved by creating soft edges/transitions between
 uses within the community and utilizing pedestrian pathways and open space areas to provide
 physical connections as well as connect to the trail system.
- Harmonious architectural, landscape, and streetscape themes as described in these design
 guidelines will be used to provide a cohesive sense of unity within the site and link the independent
 land uses.
- A comprehensive system of pedestrian sidewalks, pathways, and open space will be designed to interconnect each land use component in order to create a pedestrian friendly environment throughout the site.

Development Pattern:

Village Planning & Village Center Development Pattern: "through the 2040 Vision Strategic Planning process, citizens envision Maricopa developing as a collection of multiple village areas each with mixed-use Village Center cores anchored with commercial and employment centers served by restaurants, retail shopping and cultural opportunities such as art districts and galleries".





The development, Maricopa Stonegate, follows the development pattern as stated above. The plan is organized with a core of mixed uses, restaurants, retail, entertainment, etc. to serve the surrounding master planned communities of Glennwilde, Senita, and Santa Rosa Springs. This "village" will also include single family small lot subdivision and high-density housing. Together, Maricopa Stonegate will function as a village with housing, retail, restaurants, offices, etc. in a walkable, pedestrian friendly environment.

Purpose and Intent:

This Major PAD Amendment replaces the existing PAD from 2008 and its subsequent amendments, case nos. PAD19-02, PAD20-05, PAD23-05. The purpose of this PAD Amendment is to continue to allow for conceptual zoning of land parcels into planned uses for future development while removing/modifying the 2020 previously shown "spine road." The intent is to allow for both specific, site plan zoning for immediate project development, as well as conceptual development plans with specific site plans deferred to the time of future project development and reviewed/approved via the City of Maricopa's Development Review Permit process. The goal is to establish an experience for those living and utilizing the retail/commercial uses. Conceptually, this village experience will be created by a "truncated main street/focus and internal pedestrian trail connection" and placing, where appropriate, buildings to the property lines. The goal is to encourage community interaction and a healthy environment for pedestrian use.

The property includes Pinal County Assessor's parcels 512417200, 512417210, 512417220, 512417230, 512417240, 512417250, 512417260, 512417270. These standards will outline planning, landscaping, and architectural criteria for each area of the Planned Area Development. The criteria will help to maintain a consistent theme throughout, with similar design elements that meet or exceed the City of Maricopa's zoning ordinance. The guidelines will direct the appearance and planning of each area to make an individual but congenial impact on the overall appearance of the entire site.

The project's goal is to develop a timeless atmosphere that will fit comfortably into the existing neighborhoods of single-family homes and multi-family units that surround the property. The project is designed so commercial, offices, retail shops, restaurants, and residential are planned as one campus. The development will include pleasant pedestrian-scapes that encourage walking, mingling, and becoming a part of the scene not just a visitor. Convenient vehicle access and parking is provided throughout to encourage those residing in neighborhoods outside the comfortable walking distances to come and be a part of the scene also.

Separate freestanding pads will be developed along the Stonegate Road, and the internal streets. Larger commercial type properties will line this internal street. These pads will be complimentary to the

surrounding campus buildings in color, materials and design details. The placement of buildings on the pad is to be in harmony with a "main street" concept and should not detract from other campus buildings in terms of scale, proportion, or visibility.

Existing Conditions:



Property Description:

The property comprising Maricopa Stonegate is located along the northern side of the Maricopa-Casa Grande Highway, northwest of Stonegate Road and bordered by Alan Stephens Parkway to the northeast.

Current Use of Site:

The site is currently vacant and has historically been used for agriculture, especially at the north end of the property. There are few remnants of the native flora left on the property. Due to the property's agricultural history, the topography of the site is relatively level and there are no existing site conditions that adversely impact the development of this property. The property is located in Flood Zone "X". In 2023 Home Depot was approved at the immediate corner of Stonegate Road and Maricopa-Casa Grande Highway and proposed to be under construction in 2024.

Surrounding Area:

The surrounding master planned communities include:

North: Glennwilde

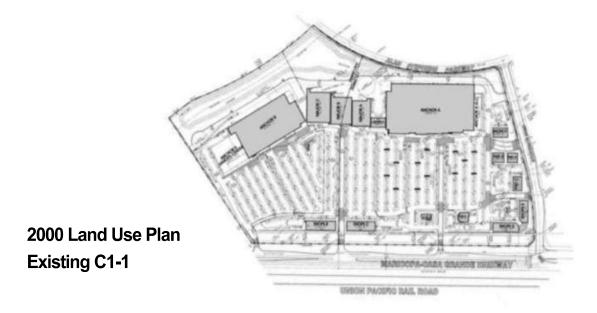
East: Commercial Development and Desert Passage master planned community

South: Maricopa-Casa Grande Highway and the Santa Rosa Springs master planned community

West: Senita master planned community

Except for the commercial development directly east across Stonegate Road, the property is surrounded by residential communities.

Existing and Proposed Land Use: Current Zoning: C1-1



Historically, the property was zoned for Multi-family Residential. In the mid-2000s, a General Plan Amendment, and PAD overlay were put in place for Industrial / Warehouse development. The land was subsequently rezoned to C1-1 for Light Industry and Warehouse Zone.

General Site Conditions: The subject site is located at the Northwest corner of Maricopa-Casa Grande Highway and Stonegate Road. The site is bordered by three public streets: Maricopa-Casa Grande Highway along the southwesterly border, Stonegate Road along the southeasterly border and Alan Stephens Parkway along the northeasterly border. The subject property currently consists of a net developable area of 46.30 acres. The site is currently vacant. The subject site has a relatively flat slope from southeast to northwest at an average slope of 0.36 percent. The site has single family residential bordering it's northwesterly side and across the Alan Stephens Parkway, it's northeasterly border. A development currently exists on the east side of Stonegate Road.

Previously Proposed Land Use Plan: Requested Zoning PAD



PAD General Mixed Use. This District is intended to allow for either horizontal or vertical mixed-use development along key circulation corridors in the City where height and density can be easily accommodated. Ground-floor retail and upper-floor residential or offices are the primary uses, with retail, personal and business services, and public and institutional space as supportive uses. These are intended to become vibrant, highly walkable areas with broad, pedestrian friendly sidewalks, trees, landscaping, and local serving uses with new buildings that step down in relationship to the scale and character of adjacent neighborhoods.

The development of Maricopa Stonegate will fulfill the General Plan requirements of a "village" which will service the surrounding master-planned communities. The 2040 General Plan gives great emphasis on the interconnectivity of new development with the surrounding community.

The benefits:

- Accessible mixed-use center with amenities useable by the community by pedestrian, bicycle, and auto.
- 2. Creating a "people place" where auto traffic is not the primary means of transportation
- 3. Creating a diverse sustainable development
- 4. Creating a village with a mixture of single family, multi-family, and commercial development easily accessible and walkable from the surrounding residential communities.
- 5. Diversity and changeability: As shown on page 9, Proposed Land Use Plan, the proposed development is diverse and will have the added ability for change to meet the constantly changing needs of the community. While all the uses shown are conceptual, the core idea is to create a village. The multi-family may change to professional offices, retail, or other compatible uses. The large box retail is flexible to a variety of larger scale uses, such as a theater complex, traditional retail, or office.

The existing PAD zoning allows for a mixed use "village" development. This development will have a single family and a multi-family component. Other uses may be entertainment, retail, offices, restaurants, hotel, and convenience store with gas. These uses may be wrapped into one zoning classification (PAD) allowing flexibility and creating a "village" type development within existing master-planned communities. Previously, the property was zoned for Multi-family Residential. The land was subsequently rezoned to C1-1 for Light Industry and Warehouse Zone.

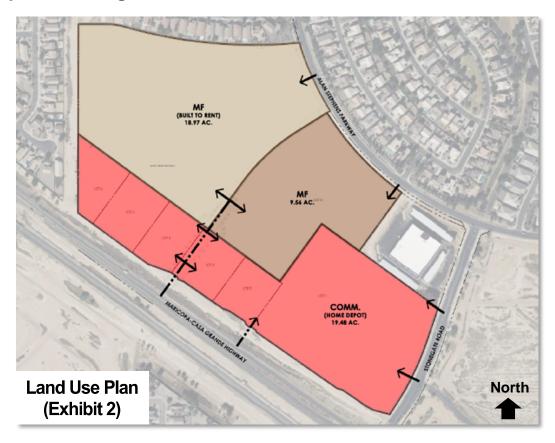
2024 Major Planned Area Development Amendment



The proposed Major PAD Amendment will allow for reduction of the "spine road" bisecting the property. The "spine road" previously approved is no longer applicable or logical with the recent approval of the Home Depot/future retail pads along Maricopa-Casa Grande Highway frontage and Stonegate Road. Dominium is proposing build-to-rent housing onsite as envisioned in 2020, and an alternative to the "spine road" is being proposed as well as modified development standards to facilitate this compatible housing type. The BTR proposal will be owned and managed by Dominium - one of the nation's largest and most innovative development companies focused on workforce housing.

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Conceptual Phasing:



It is anticipated that the development will occur in one or more phases. Development / improvement plans will be submitted to the City of Maricopa to ensure proper and orderly development with adequate access, circulation, parking, drainage, etc. for the development. Offsite improvement plans may be phased as necessary and approved/agreed to by the City and developers to allow for the opening/operation of all or a portion of the overall site.

Conformance to General Plan:

On September 15, 2020, the General Plan Amendment has been changed from "Employment" to Mixed-Use by City of Maricopa City Council.

Maricopa Stonegate Land Use Regulations Commercial and Multifamily

Use	Permitted (P)	Not Permitted (X)	Standards per City of Maricopa Zoning Code
Agriculture:		Х	
Residential:			
Single Unit Dwelling	Р		
Multiple Unit Dwelling	Р		
Senior Assisted Living & Memory Care	Р		
Group and Residential Care at a SF Residence		Х	
Supportive Housing		X	
Transitional Housing		Χ	
Public and Semi-Public:			
Community Assembly	Р		Refer to City Code for additional regs.
Cultural Facilities	Р		
Child Care Centers	Р		Refer to City Code for additional regs.
Government Buildings		Х	
Hospitals		Х	
Surgical Out-Patient	Р		
Parks & Recreation	Р		
Public Safety Facility		Х	
Social Service Facility		Х	
Bus/Rail Passenger Facility		Х	
Commercial:			
Animal Grooming Salons	Р		
Banks and Financial Institutions	Р		
Business Services	Р		
Commercial Theaters & Multiplex	Р		
Small Scale Family Entertainment	Р		
Club or Lodge		X	
Restaurants - Full Service	Р		Refer to City Code for additional regs.
Restaurants - Limited Service	Р		Refer to City Code for additional regs.
Restaurants - Drive Thru & Take Out	Р		Refer to City Code for additional regs.
Mobile Food Vender-Temporary	Р		Refer to City Code for additional regs.
Convenience Market	Р		Refer to City Code for additional regs.
General Market	Р		Refer to City Code for additional regs.
Liquor Store - Stand Alone		X	
Liquor Store - In Convenience, General, or Specialty Market	Р		
Specialty Food Sales & Facilities	Р		Refer to City Code for additional regs.
Hotel	Р		
Maintenance and Repair Services		X	
Automotive/Vehicle Repair (Minor)	P1		

Personal Services:			
Hair & Nail Salons	Р		
Barber Shops	Р		
Massage & Tattoo Parlors		Х	
Retail Sales:			
General Retail	Р		Less than 40,000 square feet ²
Art Galleries & Instruction	Р		
Outdoor Sales & Merchandise Display	P 3		
Education:			
Charter Schools	Р		
Religious Schools or Seminaries	Р		
Public Schools		Х	
Offices:		•	
Business & Professional	Р		
Medical & Dental	Р		
Walk-In Clientele	Р		
Industrial:	_		
Warehouse		Х	
Recycling Facility		X	
Manufacturing		Х	

¹ Automotive/Vehicle Repair (Minor) permitted on Lots 1-6 only.

² General Retail greater than 40,000 square feet permitted on Lot 1 (Home Depot) only.

³ Lot 1 (Home Depot) permitted outdoor display/storage in accordance with the attached site plan. Outdoor storage and display to be provided for THD moving truck rental, trailer display, shed display, HD Equipment Rental, and Seasonal Sales. Occupied parking spaces are not included in required parking count.

Parking Regulations

Use	Minimum Parking Requirement
	Minimum Farking Requirement
Residential Uses	
Single Unit	T
Single Unit Detached	2 covered spaces per dwelling
Single Unit Attached	2 covered spaces per unit
Second Dwelling Unit	1 additional space
Duplex	2.1 spaces per dwelling unit
Multiple Unit Dwelling and BTR	Guest— 0.2 space per unit Studio— 1 space per unit 1 Bedroom— 1.5 spaces per unit 2 Bedroom— 2 spaces per unit 3 Bedroom— 2.5 spaces per unit 4 Bedroom— 3 spaces per unit
Senior and Long—Term Care	0.3 space per dwelling unit plus 2 additional spaces
Residential and Group Care Home	1.0 space per dwelling unit plus 2 additional spaces. Required spaces may be tandem
Public and Semi-Public Uses	
Colleges and Trade Schools, Public and Private	1 space per 200 s/f of classroom + office space
Community Assembly	1 space per 125 s/f used for public assembly
Cultural Facilities	1.5 spaces per 1,000 s/f
Day Care Centers	1 space per 375 s/f
Clinic & Outpatient Surgical Facilities	1 space per 200 s/f 1 space per 200 s/f for urgent care facilities
Commercial Use Classifications Uses	
Animal Sales, Care and Services	
Animal Sales and Grooming	1 space per 250 s/f of sales/service area
Small Animal Day Care	1 space per 250 s/f of office/service space
Veterinary Services	1 space per 250 s/f of office/service space
Banks and Credit Unions	1 space per 300 s/f
Convenience Store and Service Station	1 space per 250 s/f of convenience retails sales
Business Services	1 space per 375 s/f
Commercial Entertainment and Recreation	
Small Scale Facility	1 space per 250 s/f or 10 spaces + 1 per 200 s/f in excess of 1,000 s/f
Theaters	1 space per 3 seats
Eating and Drinking Establishments	1
Bars and Lounges	1 space per 100 s/f of customer seating area and 1 space per 400 s/f for outdoor seating area
Restaurants, Full Service	1 space per 100 s/f of customer seating area, and 1 space per 400 s/f for outdoor seating area
Restaurants, Limited Service	1 space per 100 s/f of customer seating area, and 1 space per 400 s/f for outdoor seating area
Restaurant, Take Out Only	1 space per 300 s/f of customer seating area, and 1 space per 200 s/f for outdoor seating area
Food and Beverage Sales	
Convenience Market	1 space per 300 s/f
General Market	1 space per 300 s/f
Specialty Food Sales and Facilities	1 space per 300 s/f
Funeral Parlors and Mortuaries	1 space per 100 s/f used for public assembly plus 1 space per
Live-Work Quarters	1 space for reach 750 s/f of residential area, minimum of 1 space per unit
Lodging	<u> </u>

Hotel and Motels; Bed and Breakfast; Inns	0.8 space per room or suite of rooms with individual exits plus ancillary use requirements
Office	
Business and Professional	1 space per 375 s/f
Medical and Dental	1 space per 200 s/f
Outdoor, Temporary, and Seasonal Sales	1 space per 375 s/f display area, but not less than 4 spaces per use 1
Personal Services	1 space per 375 s/f
Retail Sales	
General, Small Scale (under 30,000 sf)	1 space per 300 s/f
General, Large Scale (over 30,000 sf)	1 space per 250 s/f plus 1 per 5,000 s/f of outdoor display area ²
Industrial Uses	
Artist's Studio and Production	1 space per 500 s/f but not less than 2 spaces per use

¹ Lot 1 (Home Depot) exempt from parking requirements for Outdoor, Temporary, Seasonal Sales parking requirements.





² Lot 1 (Home Depot) required 1 space per 275 s/f of indoor retail area, 1 space per 375 s/f of garden center area, and 1 space per 5,000 s/f of outdoor display area.

Development Requirements

	Diatriot				
	District Multi-Family				
	Single- Family	Multi- Family	Build to Rent (BTR)	Commercial	Additional Regulations
Lot and Density Standards					
Min. Lot Area (s/f) (minimum)	2,500	N/A	N/A	N/A	
Min. Lot Width (ft.) (minimum)	30	N/A	N/A	N/A	
Min. Lot Depth (ft.) (minimum)	80	N/A	N/A	N/A	
Max. Density (Units/net acre)	15	28	28	N/A	
Building Form and Location					
Maximum Building Height (ft.)	30	45 ^{1,2}	45 ^{1,2}	45 ^{1,2}	
1st Floor Ceiling Height (ft. clear)	N/A	N/A	N/A	12	
Residential Setbacks (ft.)					
Front	10 ¹	15	15 ^{2,8,9}		25' setback along Alan Stephens Pkwy ²
Interior Side	0	0	0 8,9		
Street Side	10	15	15 ^{2, 8,9}		25' setback along Alan Stephens Pkwy ²
Rear	5	15	15 ^{8,9}		
Landscaping Setbacks (ft.)					
Front			15 ^{2, 8,9}		25' setback along Alan Stephens Pkwy ²
Interior Side			10 ^{8,9}		
Street Side			15 ^{2, 8,9}		25' setback along Alan Stephens Pkwy ²
Rear			15 ^{8,9}		
Commercial Setbacks (ft)					
Front				10	
Interior Side				0	
Street Side				10	
Rear				15	
Screening of Parking	Υ	Υ	Y	Υ	
Other Standards					
Building Design	Υ	Υ	Υ	Υ	
Ground Floor Transparency	N/A	N/A	N/A	Y	
Outdoor Living Area (sq. ft. per)	100	50	50	N/A	
Pedestrian Access	Y	Y	Y	Υ	
Private Storage Space Additional Standards	Y	Υ	Y	N/A	
	Pofor to	City of Ma	ricona Zonin	a Codo unlos	s otherwise provided within this PAD
Exceptions to Height Limits Fences and Walls	Refer to City of Maricopa Zoning Code unless otherwise provided within this PAD				
	Refer to City of Maricopa Zoning Code unless otherwise provided within this PAD				
Lighting	Refer to City of Maricopa Zoning Code unless otherwise provided within this PAD				
Lighting Projections into Required Setbacks	Refer to City of Maricopa Zoning Code unless otherwise provided within this PAD Refer to City of Maricopa Zoning Code unless otherwise provided within this PAD				
Screening	Refer to City of Maricopa Zoning Code unless otherwise provided within this PAD Refer to City of Maricopa Zoning Code unless otherwise provided within this PAD				
Signs Visibility at Driveyeye					<u> </u>
Visibility at Driveways	Refer to	City of Ma	iricopa Zonin	g Code unles	s otherwise provided within this PAD

Landscaping for Solar Covered Parking Spaces	To encourage energy sustainability and shade, the use of solar energy collectors for the purpose of providing energy may be provided with or as a part of the installation of parking canopies. Required landscape island trees are permitted to be relocated to facilitate the installation and efficient use of solar covered parking canopies. Alternatives to the landscape islands location/design within the parking areas for solar covered parking spaces shall be provided and reviewed/approved via the City of Maricopa's Development Review Permit process. Solar canopies shall be able to project 2' into the landscape setbacks.
Multi-Family Duild to Dont (DTD)	Adjacent to Parking/Internal Roads: Primary circulation sidewalks shall be a minimum of 6' wide, unobstructed, hard surfaced, and offset from the face of curb by 2'.
Multi-Family Build to Rent (BTR) Sidewalk Design Standards	All Other On-Site Sidewalks: All other on-site sidewalks shall be a minimum of 4' in width.
	All internal sidewalks shall be provided and reviewed/approved via the City of Maricopa's Development Review Permit process.

Exceptions:

- (1) Garages shall be a minimum of 20' setback. Porches used for outdoor entertainment and sitting at front of residence shall be 8'-0" in depth minimum and may be setback 10' from the property line. Decorative entry porches without sitting capability but used only for entrance to the living space may be setback 10' from the property line. Casitas which are separated from the main residential dwelling by an open courtyard or walkway may be setback 8' from the property line. Zoning administrator may waive built to line setbacks based on use.
- (2) Building heights shall not exceed 45'. Multi-family abutting Alan Stephens Parkway within 30' of the property line shall have a maximum height of 25' measured from the top plate or 30' to the center of the roof slope or top of parapet. Elsewhere, multi-family shall have the ability to exceed the maximum height if 4 story buildings are utilized.
- (3) Lots 1 6 required one (1) landscaped parking island for every ten (10) contiguous linear parking spaces.
- (4) Lot 1 (Home Depot) permitted up to 500 parking spaces in one group or cluster.
- (5) Lots 1 6 require six (6) bicycle parking stalls per building.
- (6) Lot 1 (Home Depot) Minimum 4' landscape buffer from residentially zoned property required.
- (7) Lot 1 (Home Depot) Landscaping of parking lot islands and site area not required adjacent to buildings.
- (8) Encroachments within the building and landscape setbacks within the BTR for walls to enclose rear/side yards and any landscape setbacks may be encroached with walls/fencing, decorative lighting fixtures, architectural embellishments, gates, pedestrian amenities, signage, pedestrian/vehicular entry focal points and shall be reviewed/approved via the City of Maricopa's Development Permit Review process.
- (9) Partial view wall for the residences and/or full view fencing or opaque fencing/walls at the open space area(s) adjacent to the community western trail corridor and the internal pedestrian trail connection as reviewed/approved by the City of Maricopa's Development Review Permit process.

Design Guidelines

General Concept:

Architectural forms shall be visually interesting and aesthetically pleasing. The concept of a pedestrian friendly, neighborhood center should be taken into account when designing the building elevations. All building elevations should relate to one another to form a cohesive whole. Materials, colors, and finishes shall be varied yet coherent and build on a theme of "traditional" architecture. Elevations should employ the use of architectural features such as, overhangs, shade awnings, pop-outs, trellis structures, arbors, color, wrought iron grillwork, and the integration of signage into the overall design of the building. Shaded walkways and structures are encouraged. All rooftop and on grade mechanical shall be screened per City of Maricopa design standards.

Building Styles:

Warm Contemporary and Heritage Mix: Diversity of Style

Warm Contemporary balances the uncluttered qualities of modern architecture and the historical qualities of Heritage, with the use of stucco/EIFS, stone, wood, brick, or other richly textured materials. The basic elements are represented by simple and clean lines. Buildings shall have neutral color pallets with bold accent colors. Exposed and painted steel elements enhance the "modern" feel of this building style.

The following photos are examples of building styles that are acceptable.























Retail buildings, with parapets, may have exposed downspouts on the rear of the building, designed to be integrated into the overall elevation. All other commercial buildings, with parapets, are encouraged to utilize internal roof drains and drain leaders.

Multi-family buildings shall incorporate varied rooflines by utilizing gables, hipped roofs, clerestory, or chimneys, to create varied and interesting elevations. A pallet of different shades shall be utilized to differentiate the buildings, but also harmonize with each other (see photos above).

Building Materials and Finishes:

Cohesive and complementary design ideals for the Maricopa Stonegate PAD should be incorporated. A "kit of parts" with regards to materials/finishes may be utilized to bring elements from one user to the other. All users shall be reviewed/approved via the City of Maricopa's Development Review Permit process.

- **Concrete masonry units**: smooth, split face, fluted, integrally colored, flashed, sand blasted, and ground faced. These units should be sealed, stained, or painted unless approved otherwise.
- Precast concrete: columns, decorative surrounds, caps, etc. should be sealed, stained, or painted unless approved otherwise.
- Brick, natural stone, cultured stone, or ceramic/cementitious material mimicking wood, stone, or other material.
- Stucco or EIFS: sand finish for exteriors and soffits.
- Architectural metal/wrought iron work: painted or prefinished powder coated metal for use as awnings, arbors, decorative ironwork, etc.
- Architectural pre-finished metal: for roofs, copings, trim, and flashings.
- Ceramic or porcelain tile.
- Metal siding used as a design element
- Materials shall be resistant to fading and warping due to heat and sun exposure.

Color:

Color shall be used to create an interesting and exciting "place of being". Color is encouraged to be strong and vibrant. Earth tones are encouraged, with the overall concept of creating a unified but varied look to the complex. Color schemes are encouraged to follow secondary, complimentary, and tertiary color philosophies. Beige buildings with beige accents are NOT encouraged.

Discouraged Materials and Finishes:

- **Wood** unless used as a design element in "heavy timber" fashion, with a minimum dimension of 6 inches (nominal). T-111 or similar products are prohibited.
- **Vinyl** siding, windows, doors, or any other application or as approved via the Development Permit Review process.
- "Spanish Lace" stucco finish- or any other heavily textured finish apt to collect and hold dust and dirt.

Refuse Enclosures & Loading/Service Areas:

Refuse enclosure design shall adhere to the City of Maricopa standard details. Design of the walls shall be complimentary to the overall design of the complex. Metal gates shall meet all City standards and may be painted to match the primary color of the enclosure or as an accent.

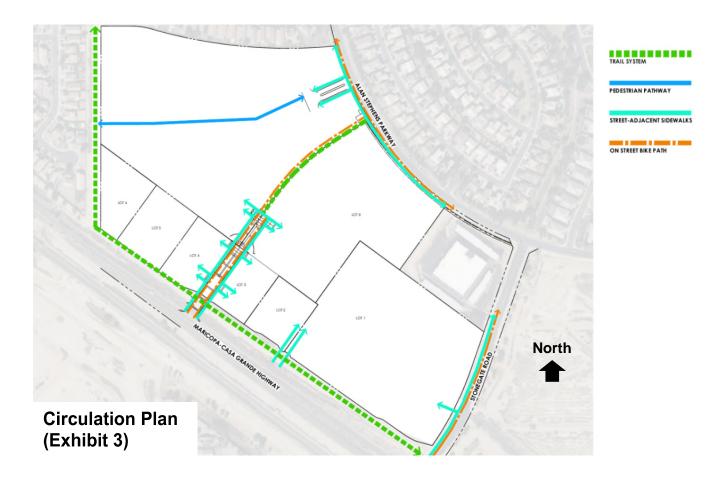
- Screening shall be complimentary to the architecture and overall site theme.
- Screening shall include walls/fences that are six feet in height.
- Located within individual parcels, clear of setbacks, and clearly marked.



Streets and Circulation

A truncated main street and internal pedestrian trail connection are provided to Maricopa-Casa Grande Highway to access the overall Maricopa Stonegate development. This truncated main street will provide a sense of arrival with the internal pedestrian trail connection completing that connection to Alan Stephens Parkway. The Dominium's BTR development will also utilize an access point onto Alan Stephens Parkway, which will limit vehicular traffic to that street.

With reasonable and appropriate ingress & egress being provided as well as an amortized internal mixed-use trail connection, the public street system should not be heavily burdened. The onsite private roads will provide internal vehicular access without the need to leave the overall development center. Also included in the circulation is the extension/accessing of the City of Maricopa trail corridor along the western boundary and the Maricopa-Casa Grande Highway. This trail system will allow the interior sidewalks and streets to connect and all work together to help move residents around in a safe and pleasant way. The overall design of these improvements shall be reviewed via the City of Maricopa's Development Review Permit process.



Grading and Drainage:

Runoff generated within the project boundary will be directed as overland flow towards the interior drive aisles. Runoff within the drive aisles shall be conveyed by curbs and gutters and collected/conveyed by a storm drain system to the onsite retention basins/underground retention systems.

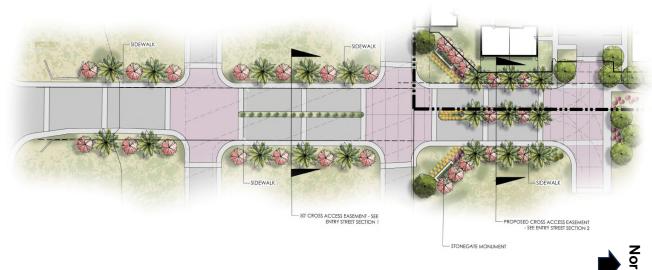
Each of the individual parcels shall be responsible for grading, drainage, and retention within their individual parcels. That is, there will not be a central retention basin, but rather individual, on-lot retention systems. On-site retention will consist of landscaped surface basins as well as sub-surface retention reservoir systems and shall be reviewed/approved by the City of Maricopa.

Water, Sewer & Dry Utilities:

Water is available along Stonegate Road and Alan Stephen Parkway, with both roads having 12" diameter ductile Iron Watermains. Global Water Resources is the franchised water provider for the area and indicated they have the capacity to serve the proposed development. At each phase as constructed will provide a current third-party certified flow test. Water stubs currently exist at the internal street connection points along Stonegate Road. It is proposed that a 12" public water main be extended through the site, within both interior streets, to provide the potable water, landscape irrigation and fire protection needs of all parcels. A 12" diameter sewer line exists in Alan Stephens Parkway, near the northwest corner of the site. This 12" diameter sewer is located at the lowest elevation of the site and is approximately 12' deep at the estimated point of connection. The sewer provider is also Global Water Resources. Dry utilities (Electric, telephone & communications) exist at or near the proposed site with connections anticipated to be routine.

Landscaping

The interior collector streets have been designed to discourage cut-through traffic, while still providing a straightforward and effective way to convey residential uses proposed onsite and retail/commercial traffic around and through the site. As shown, the truncated main street will provide a sense of arrival as well as interconnectivity between the residential uses of the site to the retail/commercial services. Street crosswalks leading from sidewalks be identified for safety and to meet ADA guidelines.



Entry Drive Enlargement (Exhibit 4)



Entry Street Section 1 – Maricopa-Casa Grande Highway Entry (Exhibit 5)





Entry Street Section 2 - Stonegate Monument (Exhibit 6)





Regional Trail Head Portal (Exhibit 7)





City Trail Connection (Exhibit 8)







Open Space Landscaping

A conceptual Landscape Plan has been provided with this submittal that indicates the planting schemes and theme. The plan proposes a plant palette which includes climate adapted plant species that will provide both seasonal flowering of plants as well as the seasonal leaf color that a couple of the trees will provide in the Fall. A thoughtful and responsible use of turf limited at focal points in the common open space of the development and to enhance the landscape "curb appeal" has been integrated into the landscape concept. All plants used for the project are from the Arizona Department of Water resources low water use plant list.

To provide a sense of arrival and identification to the planned development, a landscaped sign monuments is located the points of access and entry to the development specifically, the shared driveway connection to Maricopa-Casa Grande Highway.

The outdoor passive pedestrian spaces and areas used for outdoor dining for the retail service and restaurant parcels will be enhanced with decorative paved surfaces, site furniture including benches, planter pots, and trash receptacles; shade trees in cast-iron tree grates; raised planters with flowering shrubs and groundcovers; and decorative seat walls. For the most part, plantings used for the commercial retail and residential parcels will be the same as the plant palette used in the development of common open space as identified in this booklet, with a few additional plant species to add to variety in the landscape.

Plant Palette

The following are potential plant materials and should be continually reviewed/added to facilitate a well-designed landscape plant palette to fit with the proposed development users' needs as well as the environment(s).



TREES: Phoenix Dactylifera Date Palm

20' Trunk, Matching Specimens Diamond Cut Trunks



Washingtonia Robusta Mexican Fan Palm

26' Trunk, Matching Specimens Straight, Skinned Trunks



Pistache Ch. "Red Push" Red Push Pistachia

36" Box Minimum 12' Ht., 6' Sp., 3" Cal., Matching



Ulmus Parvifolia Evergreen Elm

24" Box Minimum 9' Ht., 4.5' Sp, 11/2" Cal., Matching



Quercus Virginiana Southern Live Oak

24" Box Minimum 9' Ht., 4.5' Sp, 11/2" Cal., Matching



Chitalpa Tashkensis Chitalpa

24" Box Minimum 8' Ht., 4' Sp, 11/4" Cal.



Olea Europaea "Wilsonii" Fruitless Olive

36" Box Minimum, Multi-Trunk" 10' Ht., 6' Sp.



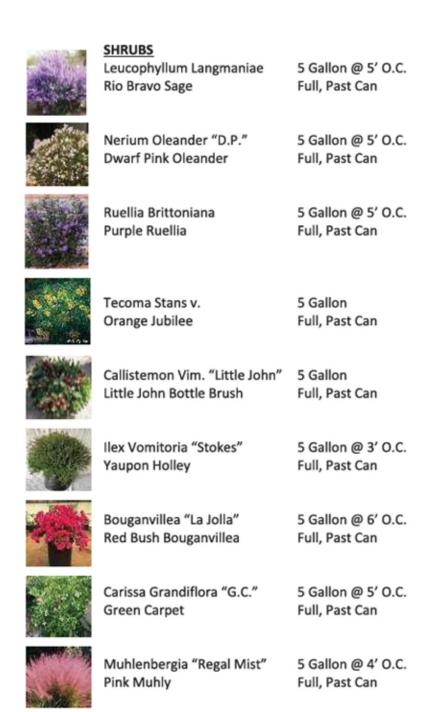
Acacia Mulga Mulga Tree

24" Box Minimum 8' Ht., 4.5' Sp., 11/4" Caliper



Acacia Salicina Willow Acacia

24" Box Minimum 9' Ht., 4.5' Sp., 11/4" Caliper





ACCENTS:

Hesperaloe Parvifolia 1 Gallon Red Yucca Full, Past Can



Agave Gemniflora Twin Flower Agave 5 Gallon Full, Past Can



Caesalpinia Pulcherrima Red Mexican Bird Of Paradise 5 Gallon Full, Past Can



GROUNDCOVERS:

Lantana Montivedensis "N.G." 1 Gallon @ 4' O.C. New Gold Lantana Full, Past Can



Lantana Sellowiana "T.P." Trailing Purple Lantana 1 Gallon @ 4' O.C. Full, Past Can



Eremophylla Minginew Gold Outback Sunrise 1 Gallon @ 6' O.C. Full, Past Can



Acacia Redolens "D.C." Desert Carpet Redolens 5 Gallon @ 7' O.C. Full, Past Can

Conclusion

The City of Maricopa has invested significant infrastructure resources towards encouraging and providing a sustainable, stable, and long-term residential component throughout the City as well as retail/commercial, employment, recreational, and transportation components. The proposed Stonegate PAD Amendment will allow for additional residential developments that will capitalize on and broaden the residential types and living opportunities offered within this immediate area as well as support existing retail/new businesses moving to the area of the City.

The proposed Stonegate PAD Amendment will facilitate a new mixed residential rental development meeting the community's needs within the City of Maricopa. The ownerships are excited to bring this mixed housing development to this vacant, infill site. By removing a vacant site (i.e., also an attractive nuisance) and replacing it with needed, and diverse, rental residential housing will help transform the neighborhood and provide accessible homes so desired. In summation, this proposed development will fulfill both the needs of the City of Maricopa and the immediate area in many ways (i.e., new development, new housing type, reinvestment, quality, increased revenue, and greater security). The proposed development standards within the specifically for Dominium along with the final design will provide a buffer and transition that mitigates any potential impacts to the existing built environment.

In conclusion, the purpose of this PAD Amendment Design Guidelines is to continue to guide the development of Maricopa Stonegate. The intent is to allow for both specific, and immediate project development, as well as conceptual development plans with specific site plans deferred to the time of future project development.